

WEST TEXAS STREET MASTER PLAN

City of Fairfield

David L. Gates & Associates
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West Texas Street is a mile-long corridor that links Interstate 80 to the historic downtown of Fairfield. The street was

“commercial strip” serving travelers as well as local residents. While West Texas Street runs through the heart of dense single family and multifamily neighborhoods, it is not a good neighbor. There is too much pavement, too many cars, and unfriendly buildings that don’t contribute to the streetscape.

West Texas today is hostile for pedestrians and bicyclists. With four lanes of fast-moving traffic adjacent to five-foot-wide sidewalks, the street provides a poor pedestrian environment. Pedestrian cross walks, while signalized, are widely separated and offer few safety features like median islands/pedestrian refuges, special paving, or safety lighting. Overall the lighting on West Texas Street is also poorly oriented for the pedestrian. Overhead cobra heads light the street pavement, but there are significant gaps that are dark and uncomfortable for pedestrians (and drivers). There is a first class pedestrian and bicycle spine a few blocks to the north, the Fairfield Linear Trail. However, the link

between West Texas Street and the Linear Trail is undefined as there is no sign directing one to the trail from West Texas Street.

Further, while West Texas Street is a major gateway to the community, it does not give visitors a positive first impression. There has been some investment at the western end of the street but much of the corridor has languished. Today, many of the structures along West Texas Street are aging. Landscaping is often lacking and/or poorly maintained. Public amenities are limited. The intersection with Interstate 80 lacks landscaping, public information signs, public art, and other elements that help create a positive "sense of arrival" to the City

This Master Plan seeks to rethink the role of West Texas Street within the larger context of the City of Fairfield. More specifically, it looks at the role of West Texas Street as a gateway to Fairfield from the western approach to the city, from the Interstate 80 off-ramp and the Linear Trail. It is important that this area set the tone for Fairfield's image. In addition, the Master Plan takes into consideration new redevelopment that is slated to take place along the street corridor and provides a design framework that will ensure these new projects will create a positive image of West Texas Street and the City of Fairfield.

Streets say a lot about a city. In the "Life and Death of Great American Cities", Jane Jacobs wrote, "Think of a city, and what comes to mind? It's streets. If a city's streets look interesting, the city looks interesting; if they look dull, the city looks dull." The streets are the means by which a traveler measures a city. The street corridors introduce motorists to the community's physical reality. They also help to orient people as they move through the city. Another important consideration is the appearance of the West Texas Street corridor and its gateway from the freeway.

The long term vision for West Texas Street is based on an aesthetically pleasing and pedestrian-friendly corridor with a mixture of uses that supports a vibrant and active community. Although it is expected that the pattern of land uses in the area will evolve to include new residential uses, commercial development will continue to be the backbone of the corridor. In addition, the City envisions a corridor that supports comfortable public transit, redevelopment of existing properties over time, high density residential, and mixed-use residential/commercial projects. New

development at key locations will complement existing land uses and upgrade the corridors appearance to encourage pedestrian activity.

The key recommendations of this study include:

- Create a visible gateway presence, including visibility from Interstate 80.
- Strengthen the overall attractiveness of the corridor
- Improve and ease pedestrian circulation.

These guidelines outline a blueprint for the physical redevelopment of the Corridor to accomplish these recommendations.

In developing this plan, the revitalization of the West Texas Street Corridor is clearly a joint public/private effort. Certain improvements are best carried out in the private sector, while improvements on public property are more appropriately implemented by the public sector. The public sector is interested in supporting and working with private parties interested in improving the overall ambience and functionality of this important arterial.

SCOPE OF STUDY

This Master Plan primarily addresses the overall streetscape character of West Texas Street, with a focus on the public right-of-way, including such elements as:

- medians
- street trees
- auto and bicycle circulation
- public transit
- sidewalk widths and character
- street furniture

While this report deals with the physical and environmental quality of the public streetscape, inevitably, it must consider the design of private development, particularly the interface between the street and parking lots, driveways, landscaping, and building orientation. Discussion of standards for private improvements are included because overall revitalization of the West Texas Corridor requires the cooperative efforts of both the public and private sector.

PURPOSE

This document is a **master plan**. As such it lays out an overall vision for the street but stops short of an actual design. It provides a foundation for what should happen over time. Actual design will follow incrementally as funds for specific projects become available and economic reality allows.

The purpose of this document is to provide a vision and a design vocabulary that will lead to an overall beautification of the corridor through both public and private sector efforts. This document is also intended to provide guidance to business and property owners, developers, and City staff in understanding the community's objectives for the Corridor.

This document has been organized in seven sections plus appendix:

- The **Introduction** describes what the project is, and includes goals for what the plan seeks to accomplish and the objectives needed to reach each goal.
- **Corridor Analysis** describes the existing features of the study area and defines major issues.
- **Design Vision** describes the overall design concept
- **Public Improvements** describes general design concepts for key segments of the corridor. The design studies recommend public improvements such as plazas, traffic signals, walkways, and landscaping. These studies will need to be more precisely developed and subject to public review and comment at all stages of the design process.
- **Pedestrian Improvements** defines ways to better pedestrian, bicycle, and mass transit circulation.
- **Redevelopment Opportunities** focuses on three key sites suitable for redevelopment. Projects at these locations could set a tone and a precedent for major redevelopment throughout the corridor.
- **Design Guidelines for Private Improvements** provides recommendations for private sector development, including site planning, landscaping,

and hardscape/paving. These guidelines are meant for landowners, merchants, and developers interested in improving the visual quality of the area, and enhancing its image as a place to live and work. Each private development proposal will be evaluated on its own merits.

PROCESS

In preparing these guidelines, the consultant team worked with City Staff, MTC, and the community to:

1. Identify the positive elements of the Corridor and those elements that have the potential to make a positive contribution to the Fairfield community and corridor identity.
2. Facilitate workshops with representatives of the community to create a vision of what the West Texas Corridor can become.
3. Develop an overall vision for the area, based upon an understanding of the key components that make up the urban design structure of the Corridor. These components include the entry points, edges, and special crossing points (referred to as nodes).
4. Create standards for the development of a unified streetscape, to be incrementally implemented.
5. Define design criteria for use by public agencies for the evaluation of future development and rehabilitation to ensure consistency with the West Texas Corridor.
6. Select a palette of appropriate materials and colors, forms, architectural and landscape elements to guide property owners in the improvement of their properties.

COMMUNITY PLANNING PROCESS

City staff believed it was critical from the onset of the Master Planning process to involve the community, especially property owners and merchants. To that end, the City sponsored a series of Workshops to solicit public feedback on West Texas Street issues. The workshops were held approximately once a month. Each workshop focusing on a different subject. They were instrumental in defining the design concept for the corridor as well as identifying economic development programs. Specific workshops included:

Session 1 (January 14, 2003)

Staff introduced the Master Plan process, including goals and objectives. Participating merchants and staff held a roundtable discussion of key issues facing West Texas Street. Merchants and property owners expressed significant concerns about public safety issues and homelessness. Because homelessness and crime were identified as critical issues, staff agreed that a focused workshop would be appropriate.

Session 2 (January 28, 2003)

This workshop focused on crime, homelessness, and vagrancy. The Fairfield Police Department led the discussion, with participation by the director of the Solano Safety Net Consortium, a local agency focused on the homeless. In responding to the community, the Fairfield Police Department offered recommendations about reporting crimes and problems, no-trespassing signage, and property maintenance to discourage trespass. The representative from the Solano Safety Net Consortium recommended a "tough love" approach to the homeless that break the law. The Police Department agreed to further community outreach efforts.

Session 3 (February 25, 2003)

At this workshop, staff introduced landscape architect David Gates, who presented his analysis of design character, opportunities, and transportation issues. Merchants identified several issues of concern, including:

- Poor overall appearance of West Texas Street,
- Lack of a strong sense of "arrival" to Fairfield,
- Poor lighting-particularly near Allan Witt Park,
- Concern about median islands blocking access to and from businesses.
- Pedestrian Safety

David Gates & Associates used the list of issues in developing the Plan

Session 4 West Texas Street Walk (March 11, 2003)

Staff, property owners, merchants and Erik Prince from David Gates and Associates walked the corridor to identify problems and discuss potential solutions. The walk confirmed many previously identified issues (poor pedestrian environment, lack of attractive streetscape, need for better lighting) and also brought out the need for stronger zoning code enforcement-particularly pertaining to landscaping. There was also discussion about bus stops. The merchants agreed that some form of seating or shelter was necessary, but there was significant concern about attracting the homeless.

Session 5 (March 25, 2003)

City staff presented potential economic development programs for West Texas Street. The merchant community was generally supportive, with strong interest in proposed façade rebates and loan programs

Session 6 (April 29, 2003)

David Gates & Associates presented some Draft Design Concepts for West Texas Street, including overall design vision, gateway elements, street furniture, and pedestrian improvements.

In general, there was support for the initial design concepts. Concerns were again raised about median islands, and staff and the consultants agreed that specific locations need refinement.

Session 7 (May 27, 2003)

City Staff presented a Draft Economic Development Program.

Session 8 (June 28, 2003) West Texas Street Cleanup and Presentation of Draft Master Plan Concept

On June 28, 2003, a West Texas Street Cleanup event was held. Staff from David Gates and Associates presented the Master Plan concept to participating members of the public and the local press. In addition, members of the community, staff from the City and David Gates and Associates patrolled the West Texas Street corridor picking up trash.

Session 9 (February 11, 2004) Planning Commission Hearing

Session 10 (upcoming) City Council Hearing

USE OF THIS DOCUMENT

There are many ways to achieve the vision defined in this document. However it is important to realize that the vision will not be realized instantaneously. Rather, the improvements will be implemented by the separate and joint efforts of public agencies, private development, and volunteer organizations. The guidelines are intended to create a framework in which improvements can occur incrementally, and yet allow for continued flexibility of individual expressions, which have made the City of Fairfield distinct and memorable.

The City of Fairfield has identified the West Texas Street Corridor as a target area for redevelopment. Redevelopment will involve the appearance of both public areas and the individual properties. The City had agreed to adopt the following goals and objectives for West Texas Street:

GOALS

1. Enhance the public right-of-way.

Use gateway landscaping, improved public parks, community signage, public art, and new lighting at key locations like the Interstate 80 off-ramp, the intersection of Oliver Road and West Texas Street/Rockville Road, and Allan Witt Park. Public art, enhanced seating areas,

special paving and public signage will be designed to create a sense of arrival to West Texas Street and the Fairfield community.

2. Enhance the environment for pedestrians, bicyclists, and motorists.

Texas Street will become a safer place for pedestrians, with pedestrian-friendly lighting, traffic signals, crosswalks, widened sidewalks, and, where appropriate, median island pedestrian refuges. West Texas Street will also become a friendlier and more comfortable place for pedestrians and bicyclists, with landscaping and development patterns that encourage more pedestrian use. Bicycling will be addressed through new signage and designated "bicycle boulevards" and/or bike lanes on cross streets, even where bicycling facilities cannot be directly integrated into West Texas Street itself. Connections between West Texas Street and the Fairfield Linear Trail and The Transportation Center will be identified, clearly signed, and improved where possible.

3. Redevelop key sites.

The West Texas Street Master Plan identifies three key sites suitable for new development. These sites can be reconfigured to provide new opportunities for mixed-use development that is friendly to pedestrians, provides a community gathering place, and provide for new economic activity and housing.

4. Provide guidance and incentives for private property owners.

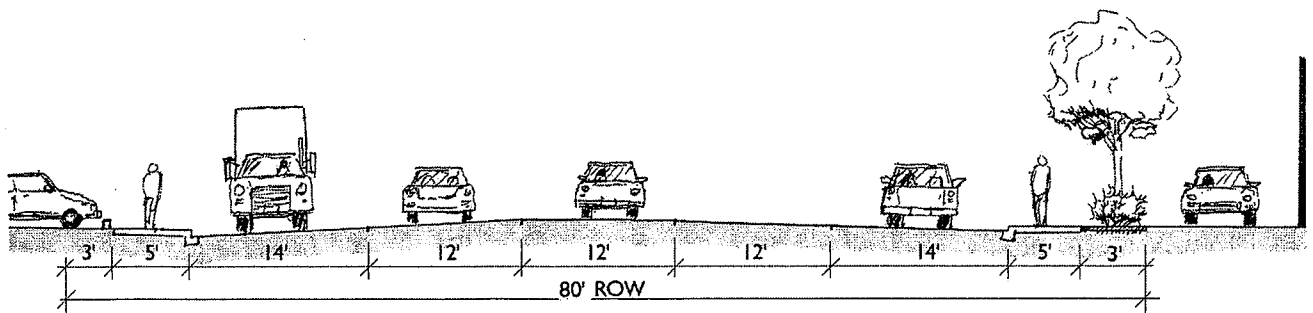
West Texas Street can only be improved through the involvement of private property owners. The Master Plan document will include guidelines for landscaping, parking lot design, pedestrian and bicycle facilities, and overall site layout. The Master Planning process will include business assistance programs to help encourage new investment that complement's the public improvements on the street.

II. CORRIDOR ANALYSIS

The West Texas Street corridor crosses the community and acts as a major arterial street connecting the heart of Fairfield to Interstate 80. West Texas Street is the first introduction to Fairfield from the Interstate 80 corridor and is the primary gateway to Fairfield and its downtown. Allan Witt Park acts as an urban refuge that resolves the commercially dominated land uses of West Texas Street.

Overall this area is characterized by significant traffic, large parking lots, an expanse of asphalt, too few street trees, a lack of overall visual continuity, and an aging architectural stock.

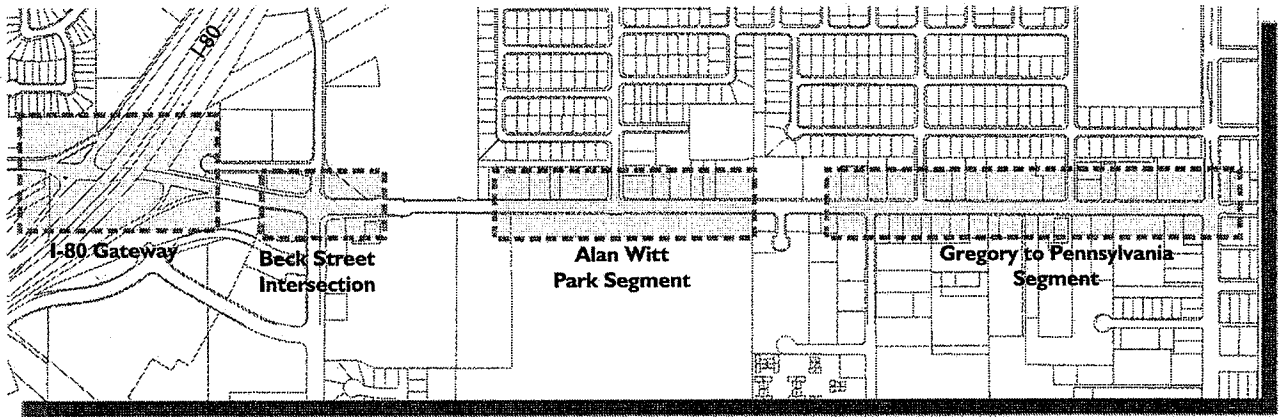
To better understand the unique setting of West Texas Street, the corridor has been divided into four zones; Interstate 80 Gateway, Beck Intersection, Allan Witt Park Segment, and Gregory to Pennsylvania Segment. Each of these components is first analyzed to determine existing conditions, and the opportunities and constraints present. Diagram 1 shows existing circulation, Diagram 2 shows site character, and Diagram 3 shows opportunities and constraints.



Section: *West Texas Existing Conditions*

West Texas Street Master Plan

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Streetscape Zones

I. Interstate 80 Gateway Zone

Because West Texas Street has a important visual presence from the Interstate 80 freeway, there is the potential and need for creating a positive impression to Fairfield. To capture the attention of I-80 travelers, design components will need to be in scale with the speed of freeway traffic but without dominating the West Texas streetscape. This area provides an opportunity to make a statement about West Texas for commuters coming into Fairfield; it will also need to accommodate and improve the circulation for pedestrians with connections to the nearby transit facility and the Linear Park Trail. At the Rockville Road intersection, there should be a clearly defined safer circulation route for pedestrians and bicycles travelling to and from West Texas on Linear Park Trail. The area beneath the Interstate 80 overpass could be improved to allow for easy and comfortable circulation, and could provide a unique opportunity for conceptual art.

Goals:

1. Create a sense of entry to Fairfield off Interstate 80 corridor
2. Create a more friendly pedestrian experience under the freeway overpass.
3. Bring both bicycles and pedestrians from the Linear Trail to West Texas Street.



Figure 1: Transit Center: important connection for pedestrians



Figure 2: Connection to Linear Park Trail at Oliver



Figure3 & 4: I-80 underpass & connection to Beck Street expresses the need for sidewalks and pedestrian connection



Figure 5: Current barriers and the need for a pedestrian connection.

Avenue, linking West Texas Street to the Linear Trail, including widened sidewalks and signage.



Figure 8: NW corner of Beck Ave.-lack of connection to Linear Park

3. Allan Witt Park Zone

The West Texas Street corridor hinges on Allan Witt Park. Allan Witt Park is a well developed, well planned public park and provides a opportunity for the community to hold events or for pedestrians to enjoy a relaxing day in the park with shade and green grass. However, the existing street frontage could be improved to enhance the recreational and leisurely qualities of the park. The park's street frontage provides an excellent opportunity to reactivate an edge of the park by adding character to the main entry point. Also in this zone is the CalTrans property which could easily be enhanced through improved landscaping and offers another opportunity to further the beautification of West Texas Street.

Goals:

1. Improve the interface between West Texas Street and Allan Witt Park through improved landscaping, signage, and pedestrian connections.
2. Extend the landscaping theme in Allan Witt Park into the Caltrans property.
3. Improve the lighting on West Texas Street in the vicinity of the park, focusing on both pedestrian scale lighting at the sidewalk and taller lighting (cobra haeaad) for the street.
4. Incorporate a fence along park frontage to channel pedestrian traffic to a central entrance to the park



Figure 9: Allan Witt Park Frontage



Figure 10: CalTrans Frontage



Figure 11: Allan Witt Park Entrance

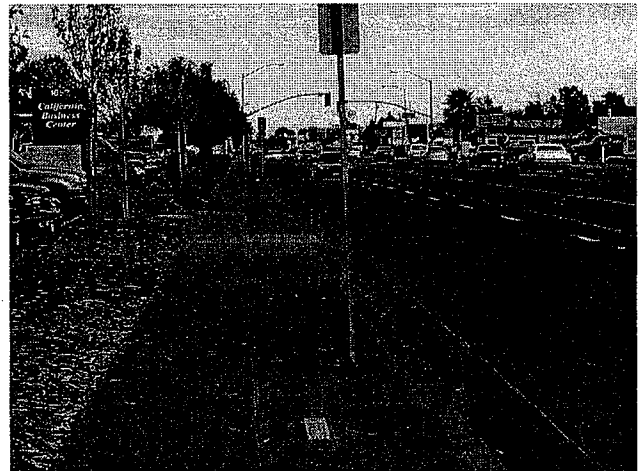


Figure 12: Typical Street Frontage near Allan Witt Park

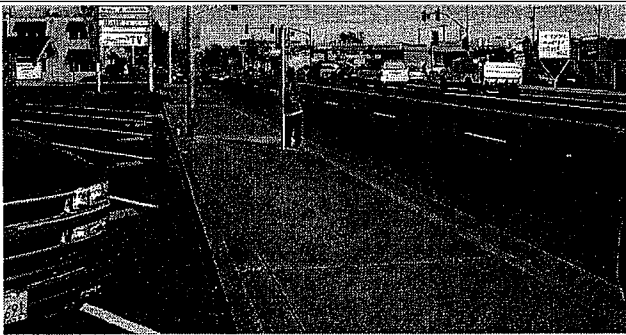


Figure 13: North corner of West Texas & Pennsylvania. No landscape setback or bus shelter, poor scale, no shade, no protection, looks inhospitable. A potential redevelopment site.

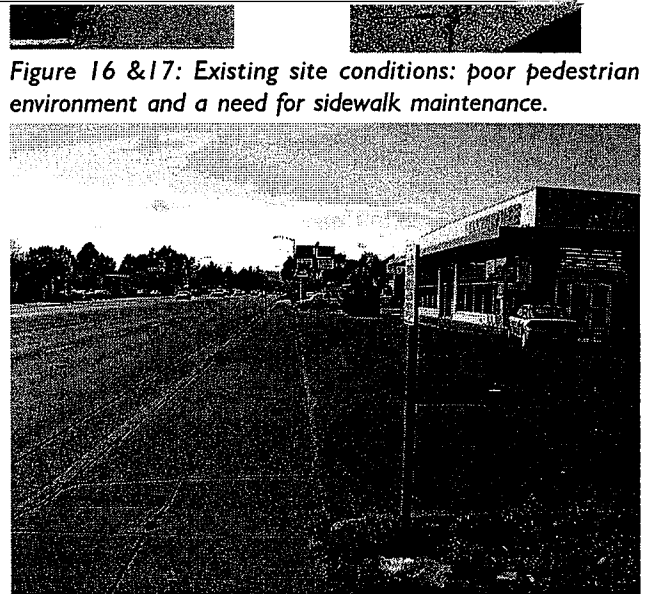
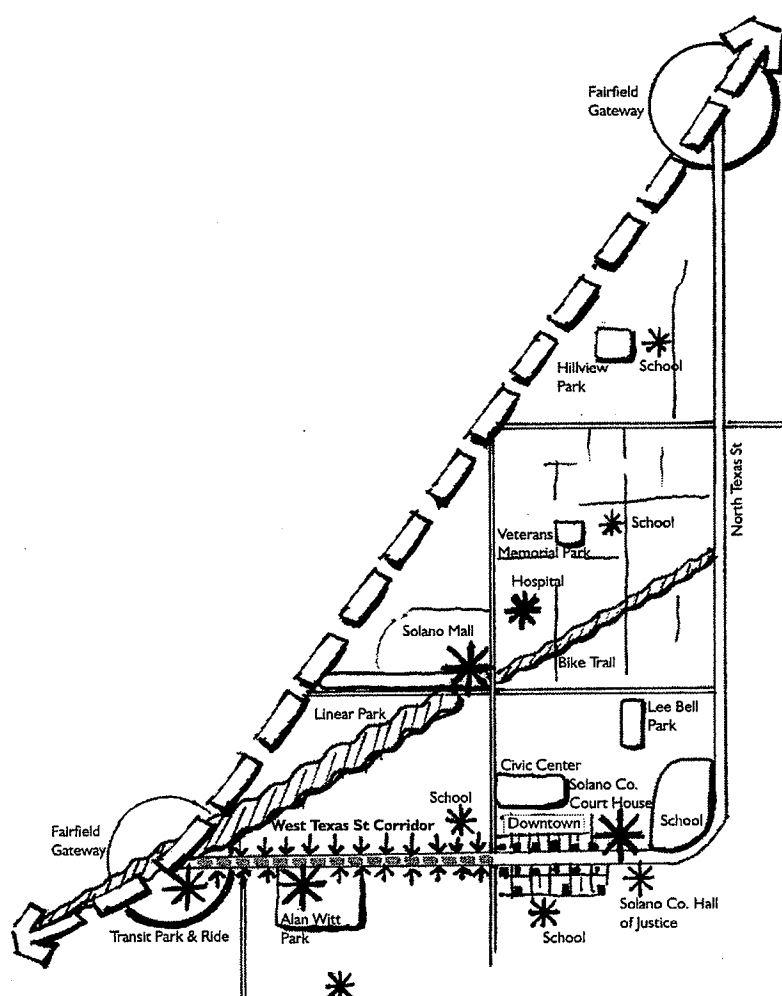


Figure 16 & 17: Existing site conditions: poor pedestrian environment and a need for sidewalk maintenance.



Figure 18: The lack of shade and separation between pedestrians and traffic makes this segment uncomfortable for pedestrians..

DESIGN VISION



West Texas Street in Relation to Fairfield Street System: The street acts as a corridor from Interstate 80 to historic Downtown Fairfield.

West Texas Street is a distinctive place within the City of Fairfield. It continues to be a central spine around which much of Fairfield is organized. The corridor contains a unique mixture of land uses and development patterns, and contains a major amenity in Allan Witt Park. The mix of uses will remain, West Texas Street will continue to carry heavy automobile traffic, support diverse businesses, and there are opportunities for introducing some mixed use. It is a crucial "link" for regional users to parks, the downtown corridor, and transit center.

West Texas Street is **not** and never will be a traditional downtown. Development patterns cover too large an area, uses are automobile-oriented, and street traffic is too heavy. However, it can become a friendlier and more attractive place for pedestrians, neighborhood residents, and school children, as well as drivers.

The corridor needs improvements to fully support its various functions. West Texas Street's history has left it with a development pattern typical of incoherent commercial strips. Street trees and other amenities are limited, and the sidewalks, driveway cuts, bus shelters, buildings, signs, and public land-uses do little to pull the corridor together or make it a

memorable place. Attractive places need human-scaled buildings and careful arrangement of streetscape elements to help create places where people want to be. This arrangement must recognize what the street is and what it can become.

IMAGE

If West Texas Street is not a “downtown,” what is it? Fairfield has stated in its General Plan, Zoning Ordinance, and design studies and policies that it wants to retain the small town, agricultural character of its past. West Texas Street should help support this image of a green, attractive “small town.”

ELEMENTS

The Design Vision recognizes the existing attractive and successful elements of West Texas Street, specifically Allan Witt Park and the new Transit Center. The Design Concept recognizes that the automobile will still be a major part of the street, but that the streetscape will be civilized and civic. The Design Vision includes the following elements:

- A more consistent “parkway strip,” with street trees, ground covers, trellises and fences, monoliths and signs, and other elements. Parking lots are softened and screened through berms, fencing, and hedges.
- Where traffic flow and safety is a serious issue, median islands may be appropriate traffic control devices. Median islands can also introduce more greenery into the center and provide refuge for pedestrians crossing the street. However, median islands would only be developed for traffic purposes where absolutely necessary and after close consultation with affected property owners and businesses.
- Attractive, well-designed gateway elements that help define West Texas Street as a special place.
- Certain focal points, or “nodes” will include a unique mixture of uses, high quality architecture, additional landscaping, and site planning that create safe centerpieces for the corridor. At such locations, pedestrian activity will be encouraged.
- The West Texas Street Corridor will be better lit, with “dark areas” filled in with new lighting.

Pedestrian scale lighting is particularly important near activity areas like the “nodes” and the park

IMPLEMENTATION

Implementing this long term concept will be accomplished through two ways:

1. Public investment at gateways, intersections, and other key locations to make West Texas Street a more memorable place.
2. Private Development: Design guidelines to implement the design vision.

Attractive public improvements at key locations can begin to help address the corridor’s visual problems. Major “image-building” projects may include monuments, public art, and special signs at key gateways and intersections. The Design Concept also identifies public street furniture that can ease and comfort pedestrians while contributing to the attractiveness of the corridor.

In implementing this Master Plan, special attention needs to be paid to maintenance of new landscaping and amenities. Private redevelopment will involve recordation of Property Maintenance Agreements with the City. Maintenance of public landscaping may require assistance from volunteers, acceptance by nearby private property owners, or even the establishment of maintenance districts. All landscaping projects will specifically address the issue of maintenance.

West Texas Street needs more coherence, greenery and shade, and more architectural interest. While public improvements can help, full redevelopment depends on investment over time by the private sector. City Design Guidelines suggest streetscape standards for new development as well as special features to identify entries to projects and add interest to streetscapes. The City is also developing loan and grant programs for facade improvements, sign programs, and property improvement.

The following chapter summarizes the overall design concept for the public sector streetscape improvements. Chapter V describes specific public improvements aimed at making West Texas a livable community. Chapter VI contains design guidelines, examples, and suggested prototypes for private improvements along the corridor. These guidelines would be implemented over time as redevelopment and development occurs.

IV. PUBLIC IMPROVEMENTS

This chapter addresses specific public improvements designed to create a unique and attractive image for the corridor and the City, and add public art and recreational opportunities to an area devoid of such amenities, and improve pedestrian safety and comfort. Except where otherwise noted, these improvements are generally located within the public right-of-way or within the required landscape setback along West Texas Street.

The proposals are set up by locations: Interstate 80 Gateway, Beck Street Intersection, Allan Witt Park Segment, Gregory to Pennsylvania Segment, and the Pennsylvania Street Intersection. The proposed improvements are intended to make a visible difference to the character of the street and be a catalyst to public and private investment and revitalization.

MEDIAN ISLANDS

Landscaped median islands can improve the safety and appearance of a street by adding shade and greenery to a street and controlling left turns and mid-block crossings. Currently, West Texas Street has no median islands. Instead, there is a shared central lane that provides left-turn access to local business. It currently has no median islands.

While median islands have many benefits, the City and the community have agreed that median islands should be planned only after close cooperation between the City, businesses and the property owners to analyze the potential impacts of the proposed medians. When studying median islands, the city should consider the following issues:

Traffic and Safety Issues

A primary reason for installing new median islands would be to improve traffic circulation. In general, traffic flow conditions and accident rates at this time do not in themselves warrant the construction of median islands. In addition, the City's traffic projections indicate that the street will generally continue to be able to carry the traffic, although congestion will worsen from today's free-flow (Level of Service "B") to heavily congested (Level of Service "D"). In the future, if traffic congestion exceed the projections, and the street or specific intersections reach "failure" (Level of Service "F"), the City may require or may install a median island if it is determined that the median island will help traffic flow. This will be determined when the city staff reviews major development projects.

In addition, there may be locations where turning movements, including U-Turns, have created a traffic flow problem that must be addressed. While mid-block turns or mid-block cross traffic do not appear at this time to create a serious traffic hazard, if such turns or traffic do create serious accident problems, the City will also consider installing median islands to improve traffic safety.

In studying the location and design of median islands, City engineers will consider the impact of any additional u-turn movements necessitated by new median islands.

Business Access and Redevelopment Opportunities

Another key issue related to median islands is the potential impact on existing business owners. In mid-block locations with frequent driveway cuts and limited access from a side street, median islands may be difficult to implement. Medians may be more easily developed in areas under redevelopment, where the City or Redevelopment Agency works with the owners of small, narrow, and deep parcels to consolidate such parcels. The project area includes blocks suitable for comprehensive redevelopment. If the City and/or project developers can mitigate the impact on other property owners, median islands could be considered during large scale projects where the number of driveways is reduced, particularly if the new design reduces the number of mid-block turns and cross traffic.

Aesthetics

Another reason for median islands would be aesthetic improvement of a block. Medians installed for aesthetic reasons will only occur where other issues (business access, traffic safety, pedestrian safety) can be addressed. This will generally occur only after working closely with affected property owners to determine ways to meet their needs (such as mid-block turning points, consolidated driveways, etc.)

Pedestrian Safety

Medians are important to pedestrian safety especially at crosswalks. They provide a place for the pedestrian to stop and wait while crossing the street. Median islands at crosswalks are recommended anywhere automobile travel lanes aren't restricted and at areas with large pedestrian flows.

V. PEDESTRIAN, BICYCLING, AND TRANSIT IMPROVEMENTS

Pedestrian comfort, safety, and ease of circulation plays a critical role in designing improvements for the West Texas Street corridor. Pedestrian circulation should be continuous and be part of a system that provides safe and easy access to goods, services, transit, and homes. Pedestrianization of the street includes improvements made to sidewalks, added connections with new sidewalks, handicap access, street furniture/resting spots, marked crosswalks, add/modify signage, lighting enhancements, bicycle safety and circulation, and transit stop treatments.

Sidewalks

Continuous sidewalk circulation is crucial to the West Texas Street Master Plan. Added sidewalks are proposed on both sides of the road from the Rockville Road intersection to the Beck Avenue intersection in order to provide pedestrians the ability to safely walk to and from West Texas Street and providing a link to the Linear Trail park system. The sidewalk will be fully accessible to pedestrians in wheelchairs with a minimum width of 5 feet. Wherever possible sidewalks should be widened (+5') at key pedestrian paths. Wider sidewalks should be from Rockville Road to Beck Avenue and the Beck Avenue access to Linear Trail and Fairfield

Transportation Center. Expanded sidewalks should also be considered along First Street, and the West Texas Street segment from Second to Pennsylvania, a major pedestrian route for students walking to the elementary school.

Relief Nodes/Street Furniture

Walking environments are enhanced by urban design elements and street furniture such as benches, seat walls, special paving materials, granite seat pads, shade, entry monuments, bus shelters, and trash receptacles. Sidewalks and walkways should keep clear of poles, sign posts, newspaper racks, and other obstacles that could block their path or become a tripping hazard. Walking areas should also be interesting for pedestrians and provide a secure environment. Good-quality street furniture that is built to last will show that the community values its public spaces. Relief Nodes at the corners of each intersection are great places to provide the pedestrian with a visual respite, incorporating calming elements like walls, monuments, shade, and special paving materials.



Figure 17: Relief Nodes.

Crosswalks

Marked crosswalks indicate locations for pedestrians to cross and signal motorists to yield to them. Marked crosswalks are desirable at locations of high pedestrian volume to guide pedestrians along a preferred walking path at intersections and other selected locations. It is important that crosswalks are visible to motorists. Patterned color concrete or inlay reflective tape are methods to identify pedestrians crossings. Other measures such as infrared pedestrian detectors, illuminated push button for signal changes, countdown signals, in-pavement lighting in the crosswalks, and audible pedestrian signals (buzzing, whistling, beeping, or chirping sounds) can safely provide a easy and comforting crosswalk

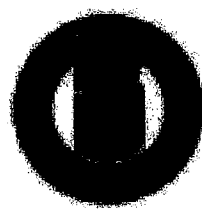


Figure 18: Illuminated crosswalks and countdown signals experience for pedestrians.

Signage

Signs can provide important information that can improve road/pedestrian safety. Regulatory signs (stop signs, yield signs, turn restrictions) require driver actions and can be enforced. Warning signs, such as pedestrian warning signs, motorist warning signs, no turn on red signs, and guide signs, make unfamiliar motorists and pedestrian cognizant of situations. Signs may also be used to prohibit pedestrian crossings at an undesirable location and reroute them to safer crossing locations. A new fluorescent yellow-green color is now approved for use on non-motorized warning signs, and attract attention because of the unique color. Signs should also be integrated into the landscape features.

Lighting

Lighting can enhance the streetscape as well as increase comfort and safety. Street lights and building lights enhance the ambiance of the area and the visibility of pedestrians by motorists. The placement of lighting should provide a consistent uniform level of light, with brighter or additional lighting at intersections.

Pedestrian-scaled lighting is also crucial to improve pedestrian comfort, security, and safety. Pedestrian scale lighting will located at key segments, intersections, and nodes such as Allan Witt Park, the Beck/Fairfield Transit Center segment, Pennsylvania intersection, and the Rockville/Linear Trail intersection.

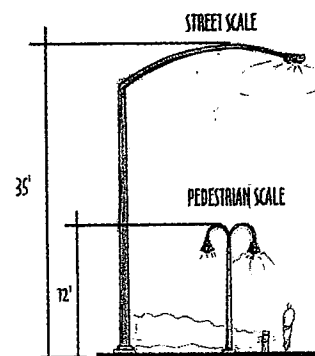


Figure 19: Street scale lights & pedestrian scale character lighting

Bicycle Circulation

Bike lanes indicate a preferential or exclusive space for bicycle travel along the street. Marking the bicycle lanes reduces the space dedicated to motor vehicles while at the same provides a buffer between motor vehicle traffic and pedestrians. The bike lanes can be marked by striping, and/or signing, or colored concrete. There are different means that accommodate bicycle travel. Class I Bikeway (Bike Path) provides a completely separated right of way for the exclusive use of bicycles and pedestrians with crossflow minimized. The only Class I Bikeway in the vicinity of The West Texas Street Master Plan is the Linear Trail. Class II Bikeway (Bike Lane) provides a striped lane for one-way bike travel on a street or highway. In general, there is insufficient space within the right-of-way for Class II Bikeways-particularly on West Texas Street proper. Class III Bikeway (Bike Route) provides for shared use with pedestrian or motor vehicle traffic. Bike Routes are recommended along Beck Avenue to allow bicycle access to and from Linear Trail and the Fairfield Transit Center (see Public Improvements section). Second, Fifth, and First Streets also. It is recommended that all Bike Routes be illuminated with proper signage. Bike Route signage should also include destination signing, when a bike route leads to a high demand destination (e.g., downtown, transit center, school, etc.). Bike Route destination and signage should be included at Fifth Street, Second Street, and First Street to direct pedestrians and bicyclists to the Linear Trail.

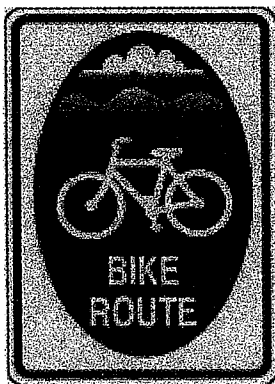


Figure 20: Bike Route Signage

Public Transit

Another essential factor to improve the pedestrianization of West Texas Street is public transportation. Good public transportation is critical to the quality of community and street life. In order to create an inviting, safe, and convenient mass transit for the community well designed transit routes and bus stops/shelters are critical. The transit stops should be designed to provide safe and convenient access and should be comfortable places for people to wait. Bus stop signage, trash receptacles, lighting, benches, and/or bus shelters are all key ingredients. It is also important to consider adequate room to load wheelchairs and important to locate transit stops on the far side of marked crosswalks to improve pedestrian safety, eliminating sight distance restriction caused by the bus. It is recommended that a transit shelter without a bench seat should be selected to prevent unwanted loitering.

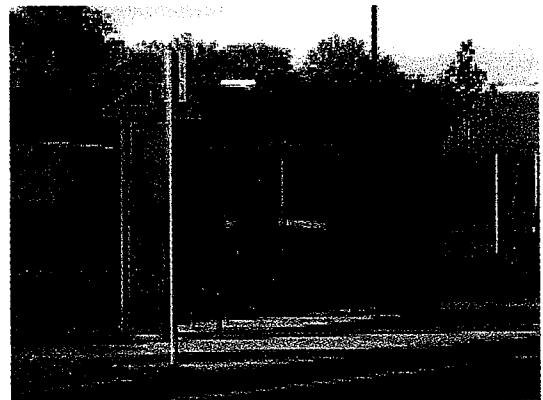


Figure 21: City of Fairfield Transit Shelters

VI. REDEVELOPMENT OPPORTUNITIES

The City of Fairfield General Plan has stated that infill and mixed-use development will be a priority of the City. With the City's aging building stock and vacant or partially vacant properties, West Texas Street can be a focal point for a new type of mixed-use development that brings a new market segment to the City, along with improving the appearance and economic health of the West Texas Street corridor. In addition, these sites should be seen as an opportunity to bring new public spaces to the corridor. The commercial spaces illustrated in these concepts are perfect for coffee shops, restaurants, newsstands, and similar uses. Such uses provide a comfortable gathering place for the community and a reason to walk from a neighborhood home to West Texas Street. When combined with pedestrian improvements such as widened sidewalks, signalized pedestrian intersections, and special signage, redevelopment of these opportunity sites could help improve the appearance, economic health, and pedestrian character of the West Texas Street corridor.

Many cities are beginning to witness the transformation of aging commercial strips similar to West Texas Street into new neighborhoods. In Berkeley, for example, several sites along University Avenue and Shattuck Avenue have new apartments and commercial space.

new urban development should be encouraged and encouraged. The SW Corner of Pennsylvania and West Texas St. (site 1) and the Indiana Avenue/Pine Tree Center area (site 3). Both areas have abutting residential uses and have vacant land and older buildings. In addition, the NW corner of Pennsylvania and Texas abuts downtown Fairfield and would be an excellent complementary location for infill development (site 2).

As part of the planning process, the City and its consultants have prepared conceptual illustrations designed to illustrate possible development scenarios for these sites.

Site 1: Southwest Corner of West Texas and Pennsylvania

This key site is a gateway to downtown, but is currently not related in any way to downtown. Pedestrians must cross a four-lane arterial with multiple turning motions to walk from downtown into West Texas Street.

The site itself is underutilized, with almost half the area vacant. The remaining buildings include ranch-style apartments and aging, single-story commercial buildings, including two storefronts used only for storage. There is no landscaping and pedestrians must cross a parking lot to get to a building entrance. The parking lot circulation is unclear, and the lot itself slopes severely. There are driveways adjacent to the intersection, creating a circulation and access problem.

New development on this site could serve as a "gateway" to downtown Fairfield and a catalyst for the redevelopment of West Texas Street's oldest segment. New development will create a landmark that provides new commercial space



Due to its location, this site shares many of the advantages and disadvantages of Site 1—e.g., it is located adjacent to downtown at a major intersection. The site is currently occupied by a small shopping center with surface parking. Parking and circulation are much better defined and maintained than that found at Site 1, and the shopping center is generally fully leased. However, redevelopment with a higher intensity mixed-use project would be appropriate.

As with Site 1, development on Site 2 will be focused on creating a more urban, pedestrian scale environment.

front portion of the site. Two smaller buildings in poor condition occupy the rear. As this site directly abuts a single family/duplex neighborhood, redevelopment with residential uses would be appropriate. Redevelopment will encourage public and private investment that improves the pedestrian environment in this segment of the West Texas Street corridor. This project will benefit from its proximity to commercial uses and Allan Witt Park.



Redevelopment on this site will be primarily residential in character. Medium density (8-15 dwelling units per acre) townhouses

of single-family homes would provide an attractive transition between commercial uses on West Texas Street and the adjacent residential neighborhood. New development could incorporate limited office uses overlooking West Texas Street. Another option is "live-work" space, with small shop or office space below and to the front, and residential uses above and to the rear.

Public-Private Partnership

Any realization of mixed-use development on these sites will require action by the property owners themselves. In the case of Site 1 (SW corner of Pennsylvania and Texas), there are several property owners involved. The City or the Redevelopment Agency can encourage property owners to develop a more urban, pedestrian friendly concept by providing property owners with technical assistance, marketing help, and design recommendations. It is important to realize that the final product would have to be fine tuned to meet zoning requirements as well as real estate market realities.

VII. DESIGN GUIDELINES FOR PRIVATE IMPROVEMENTS

To the degree that this report addresses the overall identity and appeal of the West Texas Street Corridor, its implementation will require the cooperative efforts of both the public and private sectors. In addition to the public improvements discussed, successful revitalization of West Texas Street will require improvements completed by the private sector. This Chapter provides guidelines to be followed by private development along the West Texas Street corridor. It is intended that these guidelines serve as a starting point in the preparation of development plans, with potential for individual expression.

Recommended site furniture, including lighting, benches, trash receptacles and monoliths is also provided at the end of this section.

STREETSCAPE TREATMENT

The most important element of the West Texas streetscape will be the landscaped area immediately adjoining the public sidewalk. Existing conditions in the strip of land between the sidewalk and parking or buildings vary greatly along the corridor. Some properties are fully landscaped and others

have parking immediately adjacent to the sidewalk. The goal of the West Texas Street Master Plan is for this "parkway strip" to provide a sense of continuity, scale, and greenery, and where appropriate, attractive signage. The following figures illustrate potential street cross section treatment.

1. Narrow to No Setback

Many of the existing businesses do not have sufficient setbacks from the curb to allow for the full width of landscaping recommended by this document. Unless and until the properties are redeveloped, one solution involves the use of a decorative fence planted with vines to help

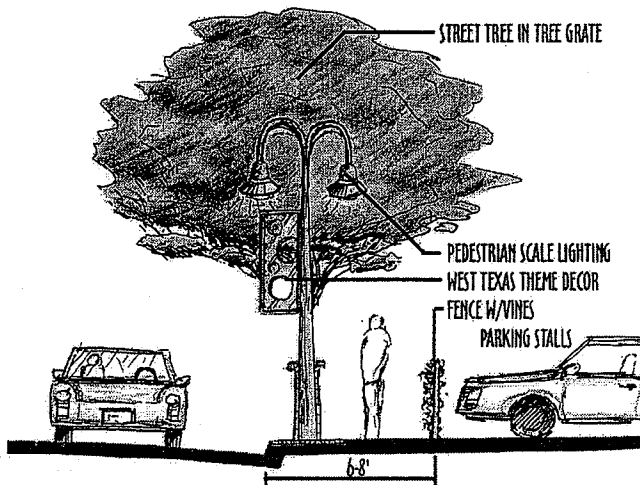


Figure 22: Section showing option for no landscape setback

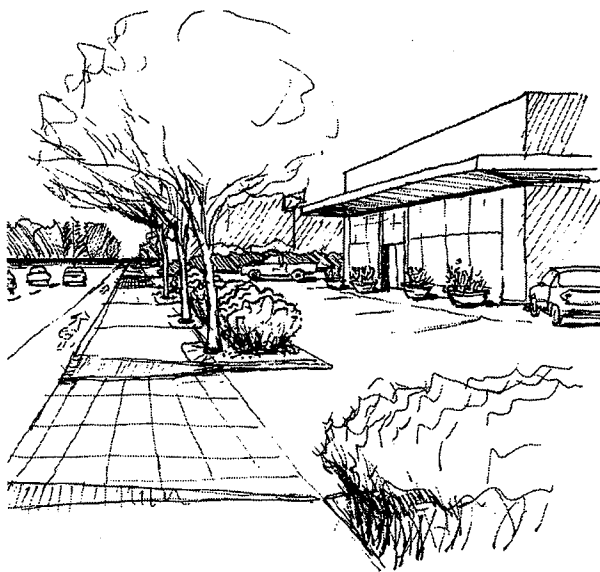


Figure 23: Sketch showing added landscape along a narrow stretch of West Texas near Pennsylvania Street. Trees half in tree grate & half in planting area in front of a low growing hedge.

screen parking while adding structure to the streetscape. In some cases, monuments may be appropriate to add further interest. In other cases street trees can be planted directly adjacent to the sidewalk or half in tree grate and half in the planting area. A small strip behind trees can be planted with a low growing hedge to separate the sidewalk from parking yet still keep visibility to businesses.

2. Deep Setback

Businesses with deeper setbacks should consider additional landscaping to better buffering street noise and traffic as well as improving business aesthetics. Consider high quality "monolith-style" signs with colorful planting near the driveway. Use low walls or berms with accent color plantings along the street. Using berms with a hedge and street trees near the top is a good, low cost way to screen parking and add color and interest to the street.

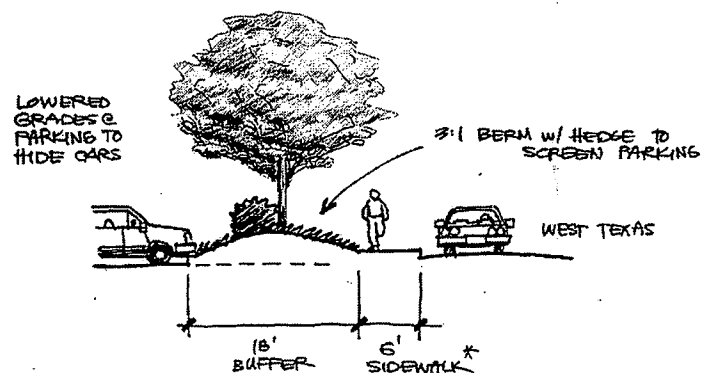


Figure 24: Section showing parking buffer with landscaped berm in cases with deep landscape

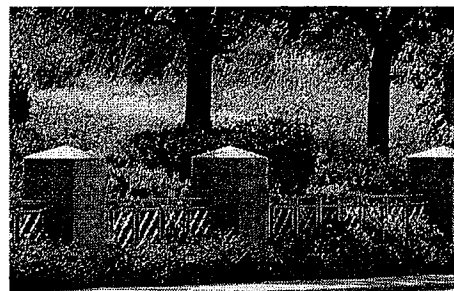


Figure 25: Examples of a monolith and fence concept around businesses landscape setback



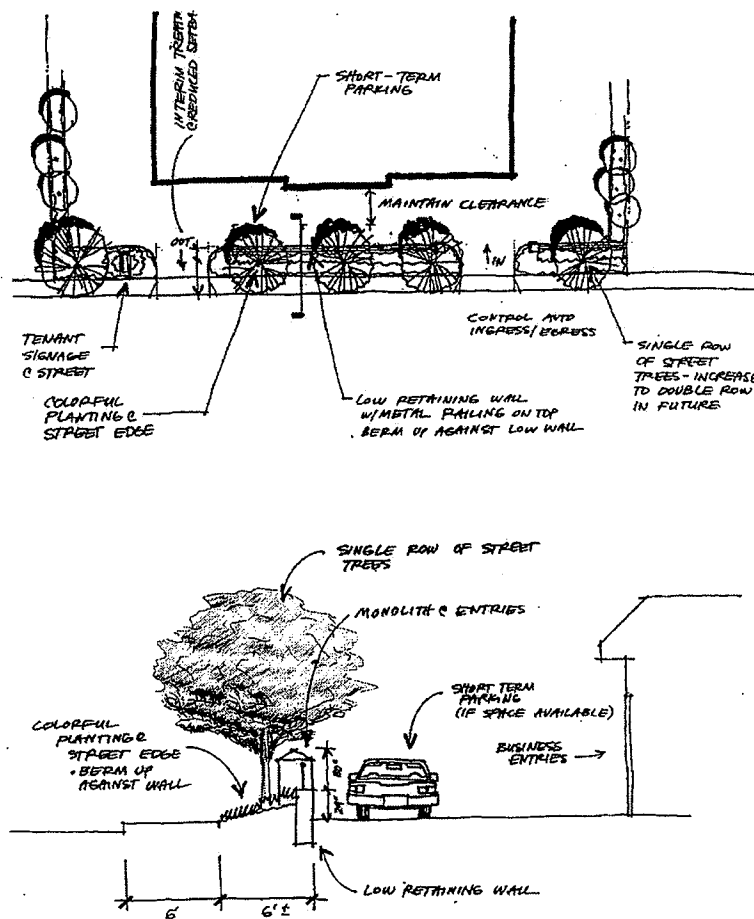


Figure 26: Typical plan & section for a small commercial business with landscape and architectural buffer

3. Retail Entry Concepts

The streetscape in front of a large retail complex should incorporate special elements that add color and interest. These special elements can strengthen the identity of the center or project, channel traffic, identify major and minor automobile entrances and provide for highly visible and unique signage along the streetscape. Corner lots and

building entries are particularly appropriate locations for such improvements. Elements that should be considered include monuments, pillars, water features, trellises, and special lighting fixtures. In certain cases, the Fairfield Redevelopment Agency may be willing to participate with the property owner in developing an especially attractive and unique street frontage.

Driveway Curb Cuts

Because of the many business along West Texas Street there are likewise many driveway curb cuts. Excessively sloped driveways, multiple adjacent driveways, and driveways that are not well-defined cause safety problems for pedestrians. To improve this existing condition, businesses could be persuaded to narrow or close driveways, convert driveways to "in/out only" movements, and provide median dividers on wide driveways. Marking driveway entrances with monoliths, planting, and business signs help pedestrians become aware of driveways.

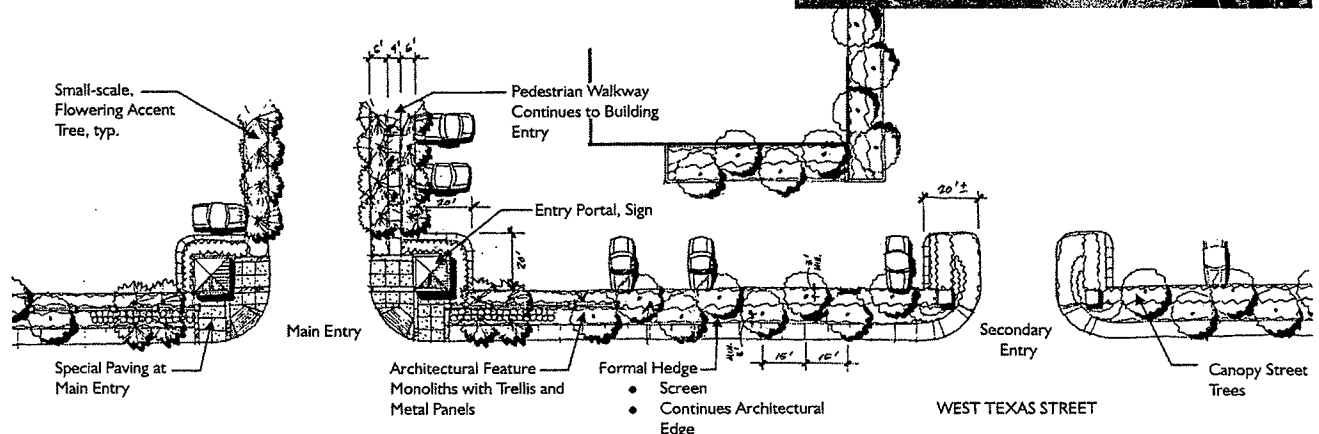
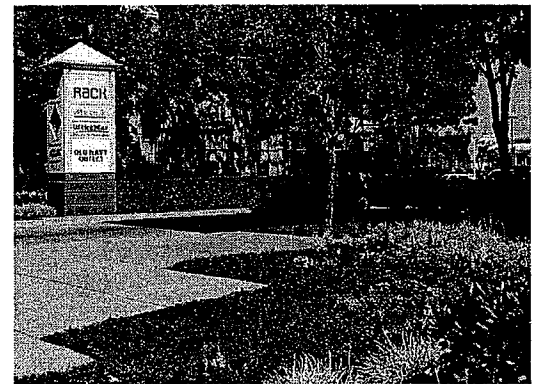


Figure 27: Typical plan of a retail entrance and frontage

STREET TREE PLANT PALETTE

These trees can be used to establish themes for individual corridors. Table A lists recommended street trees for each segment of the corridor.

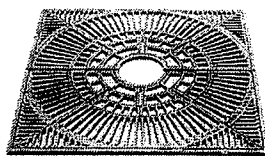
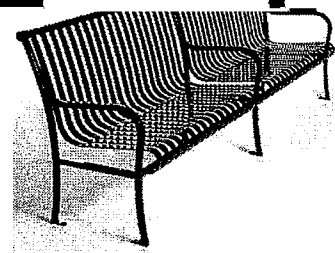
Botanical Name	Common Name	Zone					Remarks
		E	I	S	M	P	
Acer rubrum 'Red Sunset'	Red Maple			●		O	prefers acidic soils; moist conditions
Betula jacquemontii	Indian White Birch			O			infill current birch masses only
Carpinus betulus	European Hornbeam	O	O		O		not readily available
Celtis australis	European Hackberry			●		●	
Crataegus phaenopyrum	Washington Hawthorne	O					thorns a problem in pedestrian areas, messy - plant in suitable areas in groundcover
Eucalyptus nicholii	Peppermint Willow			O		O	
Fraxinus oxycarpa 'Raywood'	Raywood Ash			●			
Geijera parviflora	Australian Willow	O				O	
Ginkgo biloba 'Princeton Sentry'	Ginkgo			●		O	plant only male trees; slow
Lagerstroemia indica	Crepe Myrtle	O	O		O		use mildew resistant hybrids
Phoenix canariensis	Canary Date Palm	●				O	
Platanus acerifolia	Sycamore			●		●	Only use 'Columbia' for anthrachose and mildew resistance.
Pyrus calleryana	Flowering Pear	●	●	O	O	O	all cultivars; Aristocrat favored
Quercus agrifolia	Coast Live Oak						
Quercus ilex	Holly Oak			O	O		
Quercus lobata	Valley Oak	O					
Quercus suber	Cork Oak					O	
Quercus virginiana	Southern Live Oak				●		
Robinia x Purple Robe	Locust	O				O	not recommended near paving

key

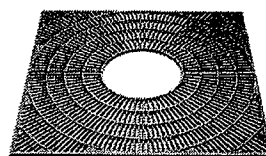
E: Entry	● Dominant Tree
I: Intersection	O Accent Tree
S: Streetscape	
M: Median	
P: Parkway / Linear Park	

interesting composition. The Forster series from Magnalite is recommended in SB85 sandblast finish, as well as the Orchard Bowl from Nichols Bros Stoneworks shown at right.

3. Tree grates should be used at pedestrian nodes and plazas for a higher quality setting. Grates should be of a natural patina finish and should meet all handicapped requirements. Recommended are Urban Accessories cast iron grates, square with light cutouts. Tree Guards are also good to consider for there aesthetic and functional protection.



Eco



*FAN

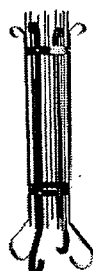


Figure 28: Granite Seat Pads



VIII. PRELIMINARY OPINION OF PROBABLE COSTS

2.	Soil prep & finish grading	79,755	SF	\$0.18	\$14,355.90
D Site Construction					
1.	Median Cobble Paving	7,000	SF	\$30.00	\$210,000.00
2.	Median Pedestrian Paving	800	SF	\$18.00	\$14,400.00
3.	Concrete Median Curb	7,700	LF	\$14.00	\$107,800.00
4.	Median Metal Fence	670	LF	\$50.00	\$33,500.00
5.	Pedestrian Signal Crossing	1	LS	\$150,000.00	\$150,000.00
6.	Colored Concrete Crosswalks	16,800	SF	\$15.00	\$252,000.00
7.	Illuminated Paved Crosswalks	2	LS	\$35,000.00	\$70,000.00
8.	Bike Route Signage	1	LS	\$8,000.00	\$8,000.00
9.	Monoliths	13	EA	\$2,800.00	\$36,400.00
10.	Repair Existing Sidewalks (25%)	12,650	SF	\$5.00	\$63,250.00
11.	Concrete Sidewalk	24,300	SF	\$5.00	\$121,500.00
12.	2' Walls	500	LF	\$125.00	\$62,500.00
13.	Gateway Feature	1	LS	\$50,000.00	\$50,000.00
14.	Metal Vine Fence	600	LF	\$75.00	\$45,000.00
15.	Ornamental Fence	100	LF	\$100.00	\$10,000.00
16.	Patterned Paving at corners	7,200	SF	\$25.00	\$180,000.00
17.	Overpass Visual Art Element	1	LS	\$70,000.00	\$70,000.00
18.	Allan Witt Park Entry Gate	1	LS	\$25,000.00	\$25,000.00
19.	Allan Witt Park Monoliths	2	EA	\$10,000.00	\$20,000.00
20.	Allan Witt Park Sign Walls	2	EA	\$9,500.00	\$19,000.00
21.	Driveway Warning Bands	550	SF	\$20.00	\$11,000.00
22.	Pedestrian Scale Lights	90	EA	\$3,000.00	\$270,000.00
23.	Festival Banners	60	EA	\$500.00	\$30,000.00
24.	Bench	10	EA	\$1,500.00	\$15,000.00
25.	Bike Rack	6	EA	\$1,000.00	\$6,000.00
26.	Tree Grates	48	EA	\$1,200.00	\$57,600.00
27.	Pot	20	EA	\$750.00	\$15,000.00
28.	Transit Shelter	4	EA	\$5,000.00	\$20,000.00
29.	Bollard	10	EA	\$250.00	\$2,500.00
E Irrigation					
1.	Irrigation	79,755	SF	\$1.25	\$99,693.75
2.	Controller	3	LS	\$10,000.00	\$30,000.00
3.	Backflow preventer	3	LS	\$500.00	\$1,500.00
4.	1 1/2" Meter	3	LS	\$0.00	\$0.00
5.	Point of Connection	3	LS	\$0.00	\$0.00
F Planting					
1.	Trees 24" Box	94	EA	\$260.00	\$24,440.00

2.	Trees 15 Gallon	111	EA	\$120.00	\$13,320.00
3.	Palms	63	EA	\$500.00	\$31,500.00
4.	Shrubs - 5 Gallon	300	EA	\$30.00	\$9,000.00
5.	Shrubs - 1 Gallon	200	EA	\$16.00	\$3,200.00
6.	Groundcover - Flats	39,100	SF	\$1.00	\$39,100.00
7.	Groundcover - Fescue	20,000	SF	\$0.50	\$10,000.00
8.	Vine	593	EA	\$35.00	\$20,755.00
9.	Root Barrier	268	EA	\$100.00	\$26,800.00
G	Maintenance Period 90 Days	79,755	SF	\$0.09	\$7,177.95
H	Subtotal Construction				\$2,876,507.16
I	20% contingency				\$575,301.43
J	Total Construction				\$3,451,808.60

cost per square foot

\$43.28

The above items, amounts, quantities, and related information are based on DGA judgement at this level of document preparation & is offered only as reference data. DGA has no control over construction quantities, costs, and related factors affecting costs, and advises the client that significant variations may occur between this opinion of probable construction costs and actual construction prices. Costs shown reflect todays dollars and no adjustments have been made for inflation/deflation in this estimate. This opinion is based on being done ate one time. Costs are based on being